



# Haverling

L O N D O N B O R O U G H

## HIGHWAYS ADVISORY COMMITTEE AGENDA

**7.30 pm**

**Tuesday  
19 March 2013**

**Town Hall, Main Road,  
Romford**

Members 9: Quorum 4

**COUNCILLORS:**

**Conservative Group  
( 5)**

**Residents' Group  
( 2)**

**Labour Group  
( 1)**

**Independent  
Residents' Group  
( 1)**

Garry Pain (Chairman)  
Billy Taylor (Vice-Chair)  
Steven Kelly  
Barry Oddy  
Frederick Thompson

Brian Eagling  
John Wood

Denis Breading

David Durant

**For information about the meeting please contact:  
Taiwo Adeoye 01708 433079  
taiwo.adeoye@haverling.gov.uk**

## AGENDA ITEMS

### 1 CHAIRMAN'S ANNOUNCEMENTS

The Chairman will announce details of the arrangements in case of fire or other events that might require the meeting room or building's evacuation.

The Chairman will also announce the following:

The Committee is reminded that the design work undertaken by Staff falls under the requirements of the Construction (Design & Management) Regulations 2007. Those Staff undertaking design work are appropriately trained, experienced and qualified to do so and can demonstrate competence under the Regulations. They also have specific legal duties associated with their work.

For the purposes of the Regulations, a Designer can include anyone who specifies or alters a design, or who specifies the use of a particular method of work or material. Whilst the Committee is of course free to make suggestions for Staff to review, it should not make design decisions as this would mean that the Committee takes on part or all of the Designer's responsibilities under the Regulations.

### 2 APOLOGIES FOR ABSENCE AND ANNOUNCEMENT OF SUBSTITUTE MEMBERS

(if any) - receive.

### 3 DISCLOSURE OF PECUNIARY INTERESTS

Members are invited to disclose any pecuniary interest in any of the items on the agenda at this point of the meeting.

*Members may still disclose any pecuniary interest in an item at any time prior to the consideration of the matter.*

### 4 MINUTES (Pages 1 - 8)

To approve as a correct record the minutes of the meeting of the Committee held on 19 February 2013, and to authorise the Chairman to sign them.

### 5 RESIDENTS PARKING ZONE RO5A - MARSHALLS PARK (OUTCOME OF PUBLIC CONSULTATION) (Pages 9 - 18)

Report attached

### 6 ORANGE TREE HILL AND NORTH ROAD, HAVERING-ATTE-BOWER - CHANGES TO TRAFFIC CALMING. OUTCOME OF PUBLIC CONSULTATION (Pages 19 - 34)

Report attached

**7 HIGHWAYS SCHEMES APPLICATIONS** (Pages 35 - 40)

The Committee is requested to consider the report relating to work in progress and applications - Report attached

**8 URGENT BUSINESS**

To consider any other item in respect of which the Chairman is of the opinion, by reason of special circumstances which shall be specified in the minutes, that the item should be considered at the meeting as a matter of urgency.

**Ian Buckmaster  
Committee Administration &  
Member Support Manager**

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# Public Document Pack Agenda Item 4

**MINUTES OF A MEETING OF THE  
HIGHWAYS ADVISORY COMMITTEE  
Town Hall, Main Road, Romford  
19 February 2013 (7.30 - 8.05 pm)**

**Present:**

**COUNCILLORS**

**Conservative Group** Garry Pain (Chairman), Billy Taylor (Vice-Chair),  
Steven Kelly, Barry Oddy and Frederick Thompson

**Residents' Group** Brian Eagling and John Wood

**Labour Group** +Pat Murray

**Independent Residents Group** David Durant

Apologies were received for the absence of Councillor Denis Breading (Councillor Pat Murray substituting) .

The Chairman reminded Members of the action to be taken in an emergency.

All votes are unanimous unless the context state otherwise.

**81 DISCLOSURE OF PECUNIARY INTERESTS**

Councillor Pat Murray declared a prejudicial interest through predetermination in item 5.

**82 MINUTES**

The minutes of the meetings of the Committee held on 8 and 15 January 2013 were approved as a correct record and signed by the Chairman.

**83 MATTERS ARISING**

Councillor Durant whilst not challenging the accuracy of the minutes sought clarification from officers that the scheme advertised in accordance with minute 76 (Parking & Loading arrangements at 69-79 Butts Green Road) was in accordance with the decision of the Committee. Officers confirmed that the advertisement was in accordance with the alternative proposal agreed by the Committee.

**84      PARKING IN THE HILLDENE SHOPPING AREA, HAROLD HILL -  
MOTION REFERRED FROM COUNCIL**

(Councillor Murray declared a prejudicial interest through predetermination and left the meeting during discussion on the item and voting thereon.)

Further to the decision to consult on plans to prevent commuters from taking up available car parking spaces, which could be used by local businesses and residents in the Hilldene Shopping Area, Harold Hill, the Committee were advised that a motion (and amendment) had been referred to this committee by Council.

The design proposed that parking areas designated on plan QJ063 /02/06 were allocated for pay and display parking areas; resident and business permit parking and disabled parking bays. Furthermore it was proposed to introduce waiting restrictions to help improve traffic flow, prevent obstructive parking and improve road safety.

The Labour Group had submitted a motion to Council:  
'This Council agrees to withdraw the Administration's proposal to introduce Pay & Display Parking and Resident Parking Permits at the Hilldene Shopping Area in Harold Hill.'

In response the Administration had submitted the following amendment:  
'Hilldene shopping area's future relies on local communities continuing to use it. Therefore this Council believes that it should work towards ensuring that future demand for car parking is protected for local residents and that commuter parking is deterred.'

The motion and amendment had been referred to this committee for consideration.

The Committee considered the report and, following debate the amendment was called:

By a majority of 5 votes in favour, to two against and one abstention the Administration amendment was agreed.

The original motion was then put to the meeting and this was lost by 7 votes against and one abstention.

**85      BUS STOP ACCESSIBILITY GUBBINS LANE AND NORTH HILL DRIVE  
2012/13 - OUTCOME OF PUBLIC CONSULTATION**

The Committee considered a report on responses to a consultation for the provision of fully accessible bus stops along Gubbins Lane, Harold Wood and North Hill Drive, Harold Hill. The three bus stops in Gubbins Lane were not contentious.

The proposal for a bus stop outside 80 to 90 North Hill Drive had attracted a response from the resident in the vicinity who used a specially adapted vehicle as they were only able to walk a short distance with crutches. Having discussed the matter with the carer involved officers had drawn up a revised scheme which accommodated these specific needs.

Although objections had been received in respect of the relocation of the bus stop from outside 98/100 North Hill Drive to outside 108 North Hill Drive/side of 2 North Hill Green officers had been unable to come up with an alternative proposal which met the requirement to provide a fully accessible bus stop.

The Committee considered the report and, following debate, **RESOLVED:**

1. To recommend to the Cabinet Member for Community Empowerment that the bus stop accessibility improvements set out in the report and shown on the following drawings be implemented;

Gubbins Lane

- QL020-OF-108A (1 stop)
- QL020-OF-109 & 110A (2 stops)

North Hill Drive

- QL020-OF-101 & 102A (2 stops)
- QL020 –OF-104 & 105B (stop outside 83 to 89 North Hill Drive)
- QL020-OF-106 & 107A (2 stops)

(Carried by 7 votes for, 1 vote against and 1 abstention.)

2. To recommend to the Cabinet Member for Community Empowerment that the bus stop accessibility improvements outside 80 to 90 North Hill Drive and shown on Drawing QL020-OF-104 & 105B be implemented, thereby reducing the bus stop clearway length by 4 metres to end at the boundary of Nos. 80 & 82.

(Carried by 8 votes for and 1 abstention.)

3. To recommend to the Cabinet Member for Community Engagement that the bus stop outside 98/100 North Hill Drive be relocated outside 108 North Hill Drive/side of 2 North Hill Green and made accessible, along with re-provision of footway parking as shown on QL020-OF-103B.

(Carried by 7 votes for, 1 vote against and 1 abstention)

4. To note that the estimated cost of £50,000 for implementation would be met by Transport for London through the 2012/13 Local Implementation Plan additional allocation for Bus Stop Accessibility for R294.

86 **ORANGE TREE HILL AND NORTH ROAD, HAVERING-ATTE-BOWER CHANGES TO TRAFFIC CALMING - OUTCOME OF PUBLIC CONSULTATION**

**RESOLVED** that consideration of the report be deferred until the next meeting pending further consultation with the Head of Streetcare.

87 **RESIDENTS PARKING ZONE RO5A - MARSHALLS PARK (OUTCOME OF PUBLIC CONSULTATION)**

**RESOLVED** that consideration of the report be deferred until the next meeting pending further consultation with the Head of Streetcare.

88 **HIGHWAYS SCHEMES APPLICATIONS**

The report presented Members with all new highway schemes requests in order for a decision to be made on whether the scheme should progress or not before resources were expended on detailed design and consultation.

The Committee would either make recommendations to the Head of Street Care to progress the scheme or the Committee would reject the request.

The Committee considered and agreed in principle the schedule that detailed the applications received by the service en bloc.

The Committee's decisions were noted as follows against each request:

<b>SECTION A - Highway scheme proposals with funding in place</b>			
<b>Item Ref</b>	<b>Scheme</b>	<b>Description</b>	<b>Decision</b>
H1	Main Road/North Street Corridor - Implementation	Upper Brentwood Road/ Main Road junction review; North Street/ A12 junction review	AGREED - Unanimously
H2	Main Road/ North Street Corridor - Feasibility	Balgores Lane/ Main Road junction review	AGREED - Unanimously
H3	Freight Loading Facilities	Programme to provide accessible loading facilities around town, district and local centres across Havering	AGREED - Unanimously
H4	Review of Lower Bedfords Road/ Straight Road junction	Continuance of multi-year programme. Links to Ambitions and Whitworth/ Broxhill sites development and S106 funding to be triggered in future.	AGREED - Unanimously



**Highways Advisory Committee, 19  
February 2013**

H5	Romford Public Realm Enhancement	Continuance of multi-year scheme to repave, declutter and landscape core of Town Centre	AGREED - Unanimously
H6	Upminster Road South Public Realm Improvements	Improvements around shipping parade near Brights Avenue. Continuance of multi-year scheme	AGREED - Unanimously
H7	Improving reliability of public transport (improving bus reliability)	Borough-wide	AGREED - Unanimously
H8	Ingrebourne Valley Connect 2 programme	Connect 2 Route, final year works	AGREED - Unanimously
H9	Bus Stop Accessibility Improvements	Havering Road and Pettits Lane North	AGREED - Unanimously
H10	Improvements to A124 cycle route - Upminster to Rush Green	A124 Corridor	AGREED - Unanimously
H11	Squirrels Heath / Ardleigh Green Road junction feasibility	Squirrels Heath/ Ardleigh Green junction	AGREED - Unanimously
H12	Improvements in Hornchurch for Taxis	Hornchurch Town Centre	AGREED - Unanimously
H13	Gubbins Lane Public Realm and Environmental Improvements Package	Gubbins Lane	AGREED - Unanimously
H14	Romford Casualty reduction package (129 accidents in 5 years)	Brentwood Road, Balgores Lane, Victoria Road, Heath Park Road	AGREED - Unanimously
H15	Rainham Package (147 accidents over 5 years)	A1306 from Dover's Corner to Thurrock borough boundary, Upminster Road North, Airfield Way, Suttons Lane	AGREED - Unanimously
H16	Collier Row Package (37 accidents over 5 years)	Collier Row Lane, Havering Road, Clockhouse Lane, etc.	AGREED - Unanimously
H17	Feasibility for 2014/15 Casualty Reduction Packages	Borough-wide	AGREED - Unanimously
H18	Collier Row to Harold Hill Cycle Safety Improvements	Chase Cross Road/ Havering Road junction, Noak Hill Road/ Lower Bedfords Road junction	AGREED - Unanimously
H19	Highway improvements to pedestrian access from Harwood Hall Lane to Oakfield Montessori School	Harwood Hall Lane	AGREED - Unanimously

H20	Highway improvements to pedestrian access to Gidea Park School from Lodge Avenue	Lodge Avenue	AGREED - Unanimously
H21	Bus turn around area for increased frequency to St Francis Hospice feasibility study	North Road	AGREED - Unanimously
<b>SECTION B - Highway scheme proposals without funding available</b>			
H22	Hainault Road, north of A12	50 signature petition to close the road to through traffic.	REJECTED 8 votes for rejection 1 abstention
H23	Marlborough Road, Mawneys	Street being used as a "mini race track" and a hump is needed to slow traffic down. Road/ A127.	REJECTED 8 votes for rejection 1 abstention
H24	Faircross Avenue, Havering Road end	7ft width restriction	REJECTED 8 votes for rejection 1 against
<b>SECTION C - Highway scheme proposals on hold for future discussion (for noting)</b>			
Nothing reported this month			

**89 TRAFFIC AND PARKING SCHEMES WORK PROGRAMME**

The report before the Committee detailed all Minor Traffic and Parking Scheme application requests in order for a decision to be made on whether the scheme should progress or not before resources were expended on detailed design and consultation.

The Committee would either make recommendations to the Head of Street Care to progress the scheme or the Committee would reject the request.

The Committee considered and agreed in principle the schedule that detailed the applications received by the service.

The Committee's decisions were noted as follows against each scheme:

<b>SECTION A – Minor Traffic and Parking Scheme Requests</b>			
<b>Item Ref</b>	<b>Location</b>	<b>Description</b>	<b>Decision</b>
	Nothing reported this month		
<b>SECTION B – Minor Traffic and Parking Scheme Requests on hold for future discussion or funding issues</b>			
<b>Item Ref</b>	<b>Location</b>	<b>Description</b>	<b>Decision</b>
TPC279	Brooklands Ward	As requested at the April 2012 HAC meeting a parking review of the Brooklands Ward was requested to be undertaken. Draft designs have been produced and are to be presented to the Committee. The proposals incorporates schemes approved for implementation.	AGREED to move to Section A of the Schedule and proceed to consultation unanimously
TPC280	Romleighs Estate	This item is based on numerous requests and reports and petitions received in recent months from both residents and Ward Cllrs of the Romleighs Estate to address the parking issues	AGREED to move to Section A of the Schedule and proceed to consultation unanimously
TPC281	The Drive, Harold Wood	Request to change the existing Disc Parking bay in The Drive to a Pay & Display parking bay.	AGREED to move to Section A of the Schedule and proceed to consultation unanimously

In response to questions from the Committee officers advised that they would be submitting a progress report on all Minor Traffic and Parking Schemes to the next meeting.

90 **CALENDAR BRIEF**

The Committee were reminded that Council had amended the constitution to extend the delegations to the Head of StreetCare to include:

- (u) To authorise the creation, amendment and removal of disabled persons' parking bays, footway parking days and at any time waiting restrictions at bends and road junctions.
- (r) To approve local highway management schemes in principle for public consultation.
- (gg) To approve or reject for further consideration proposals made to the Council for local highway management schemes in accordance with the criteria agreed from time to time by the Cabinet Member for Community Empowerment, provided that the proposal has previously been notified to members via Calendar Brief and no member has requested within 7 days of the notification that the proposal be referred to the Highways Advisory Committee for consideration.

The Constitution has been revised in respect of Part 3 Section 3.6.3 paragraphs (u) and (r) and is to be amended in respect of (gg) above. Under the revised arrangements which were approved by Council the Head of StreetCare would publish two lists on the CalBrief each week. The first list would be schemes which he did not have delegated powers to implement and would therefore need to be referred to this committee. The second list would contain details of schemes which he had delegated powers to implement and would not require a final decision from this committee. It is this second list from which members can ask for schemes to be referred to this Committee for consideration.

A detailed update will be presented to a subsequent meeting of the Committee to ensure members are fully aware of the new arrangements.

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**Chairman**

# HIGHWAYS ADVISORY COMMITTEE

19 March 2013

# REPORT

**Subject Heading:**

**RESIDENTS PARKING ZONE R05A  
Marshall Park  
Outcome of public consultation**

**Report Author and contact details:**

Mark Philpotts  
Principal Engineer  
01708 433751  
mark.philpotts@havering.gov.uk

**The subject matter of this report deals with the following Council Objectives**

Clean, safe and green borough	<input checked="" type="checkbox"/>
Excellence in education and learning	<input type="checkbox"/>
Opportunities for all through economic, social and cultural activity	<input type="checkbox"/>
Value and enhance the life of every individual	<input checked="" type="checkbox"/>
High customer satisfaction and a stable council tax	<input type="checkbox"/>

**SUMMARY**

This report sets out the comments received in response to a public consultation on proposals to provide a new residents parking zone in Caxton Way, Dickens Way and Brunel Close. The consultation followed the adoption of these roads as highway maintainable at the public expense. This report therefore seeks a recommendation to the Cabinet Member for Community Empowerment that the measures be implemented as advertised.

This scheme is within **Romford Town** ward.

## RECOMMENDATIONS

1. That the Committee having considered the representations made recommends to the Cabinet Member for Community Empowerment that the following measures be implemented as described in this report and shown on Drawing QL062-OF-101-A Final Parking Restrictions.
2. That it be noted that the estimated cost of £4,000 for implementation will be met by Taylor Wimpey North Thames Limited secured by an agreement made under Section 38 of the Highways Act 1980.

## REPORT DETAIL

### 1.0 Background

- 1.1 Dickens Way, Brunel Close and Caxton Way were adopted as highway maintainable at the public expense on the 5<sup>th</sup> July 2012. As a result of this and due to the proximity of Romford railway station and shopping centre, parking controls are being proposed to prevent both commuter and shopper parking.
- 1.2 Prior to the current proposals being formally advertised, discussions were held with the local residents' association regarding the requirements for; and the appropriate level of, parking restrictions. This discussion informed the type of restriction taken forward and its hours of operation.
- 1.3 In order to deal with the implementation of parking restrictions following the adoption of the roads, a sum of £4k has been provided by Taylor Wimpey North Thames Limited. A request to proceed with design and consultation on suitable measures was approved by the Highways Advisory Committee at its meeting of 24<sup>th</sup> January 2013 (Item H33, Highways Schemes Applications Schedule).
- 1.4 The proposed layout is shown on Drawing QL062-OF-101-A Final Parking Restrictions. The parking permit zone is proposed to be operational between 8:30am – 6:30pm Monday to Saturday Inclusive and includes all areas of Dickens Way, Caxton Way and Brunel Close other than those areas covered by yellow lines. Only residents of those roads named above may apply for residential permits. The hours of operation match the surrounding road network. The yellow line waiting restriction is proposed to be operational at any time.

- 1.5 This methodology where parking permit zones do not include lining is a new approach made possible by the amendments to the Traffic Signs, Regulations and General Directions. Lines are no longer required and signs are erected at the zone entrance with repeater signs on lamp columns. This reduces the level of maintenance required and allows residents a level of flexibility that markings do not.
- 1.6 Approximately 50 letters were hand-delivered to those potentially affected by the scheme and the immediate area on or just after 11<sup>th</sup> January 2012, with a closing date of 1<sup>st</sup> February 2012 for comments. A set of consultation information was also provided for standard consultees.

## **2.0 Outcome of Public Consultation**

- 2.1 By the close of the public consultation, 17 responses were received from members of the public;

Of these responses:

1. *14 responses were in full support of the proposals*
2. *2 responses requested reduced hours of operation with 1 requesting restrictions during school hours and the other removing Saturday from the restricted hours.*
3. *1 response requested increased hours of operation and raises the issue of restrictive covenants on the properties and the narrowness of the roads.*

- 2.2 No other responses were received.

## **3.0 Staff Comments**

- 3.1 Given the level of support for the proposals staff recommend that the scheme be implemented as advertised.

## **IMPLICATIONS AND RISKS**

### **Financial implications and risks:**

The estimated cost of £4,000 for implementation will be met by Taylor Wimpey North Thames Limited secured by an agreement made under Section 38 of the Highways Act 1990.

This is a standard project for the Council and there is no expectation that the works cannot be contained within the cost estimate. There is an element of contingency built into the financial estimate. In the unlikely event of an overspend, the balance would need to be contained within the overall Council Streetcare budget.

**Legal implications and risks:**

Parking and loading bays require advertisement and consultation before a decision can be taken on implementation.

**Human Resources implications and risks:**

None.

**Equalities Implications and Risks:**

The Council has a general duty under the Equality Act 2010 to ensure that its highway network is accessible to all. Where infrastructure is provided or substantially upgraded, reasonable adjustments should be made to improve access. In considering the impacts and making improvements for people with protected characteristics (mainly, but not limited to disabled people, the young and older people), this will assist the Council in meeting its duty under the Act.

Blue badge-holders are permitted to park in permit parking areas for an unlimited length of time and without charge.

**BACKGROUND PAPERS**

Drawing: QL062-OF-101-A Final Parking Restrictions



## NOTICE OF PROPOSAL

## LONDON BOROUGH OF HAVERING

## THE HAVERING (WAITING AND LOADING RESTRICTION) (CIVIL ENFORCEMENT AREA)

(NO. 1) (AMENDMENT NO. \*\*) ORDER 201\*

## THE HAVERING (ROMFORD PARKING PLACES) (RO5A) (CIVIL ENFORCEMENT AREA) ORDER 201\*

1. NOTICE IS HEREBY GIVEN that the Council of the London Borough of Havering, hereinafter called the Council, propose to make the above-mentioned Orders under sections 6, 45, 46, 49 and 124 of and Part IV of Schedule 9 to the Road Traffic Regulation Act 1984, as amended.
2. The effect of the Waiting & Loading Restriction Order would be to impose waiting restrictions operative at any time on the lengths of streets specified in Schedule 1 to this Notice.
3. The effect of the Romford Parking Places RO5A Order would be to:-

(a) create a controlled parking zone where residents whose postal address is indicated in Schedule 2A to this Notice may purchase permits at the following charges:-

- |                                 |   |  |
|---------------------------------|---|--|
| (a) residents permit            | - | 1 <sup>st</sup> permit £20.00, 2 <sup>nd</sup> permit £25.00, 3 <sup>rd</sup> permit and any thereafter £60.00 |
| (b) visitors permits            | - | £1.00 per permit for up to 4 hours (sold in £10.00 books of 10 permits)  |
| (c) Casual discretionary permit | - | £5.25 per permit valid for 2 hours   |
| (d) Consent to park waiver      | - | £10.15 per day for the 1 <sup>st</sup> 14 days and £15.25 per day from 14 days to 3 months                     |

(b) to provide residents parking places, operative between 8.30 a.m. and 6.30 p.m. on Mondays to Saturdays inclusive, on the lengths of streets specified in Schedule 2B to this Notice, where a vehicle displaying a valid residents parking permit may be left without time limit.

**Please Note:** There will be signs at the entrance to the controlled parking zone reading "Permit holders parking only past this point" with some repeater signs. However, there will be no parking bays marked out and residents permit holders will be able to park wherever it is safe to do so, as near to the kerb as possible (when parking parallel to the kerb) and must avoid creating an obstruction to other vehicles.

4. Copies of the proposed Orders, of the Order being amended, together with the Council's statement of reasons for proposing to make the Orders and plans showing the locations and effects of the Orders can be inspected until the end of six weeks from the date on which the Orders are made or as the case may be, the Council decides not to make the Orders, during normal office hours on Mondays to Fridays inclusive, at Traffic & Engineering, StreetCare, Mercury House, Mercury Gardens, Romford, Essex RM1 3DW.
5. Any person desiring to object to the proposals or make other representation should send a statement in writing of either their objection or representations and the grounds thereof to Mark Philpotts, Traffic & Engineering, StreetCare, Mercury House, Mercury Gardens, Romford, Essex RM1 3DW, quoting reference LBH/688 to arrive by 1 February 2013.

Dated 11 January 2013

**IAN BURNS**  
*Acting Assistant Chief Executive*

Town Hall  
Main Road  
Romford  
RM1 3BD

## **SCHEDULE 1**

### **Brunel Close**

- (a) the north-west to south-east arm
  - (i) both sides, between the south-eastern kerb-line of Dickens Way and a point 10 metres south-east of that kerb-line;
  - (ii) both sides, between the north-western kerb-line of the south-west to north-east arm of Brunel Close and a point 10 metres north-west of that kerb-line;
- (b) the south-west to north-east arm, the north-west side, between a point 10 metres south-west of the south-western kerb-line of the north-west to south-east arm of Brunel Close and a point 10 metres north-east of the north-eastern kerb-line of the north-west to south-east arm of Brunel Close.

**Caxton Way**, both sides, between the western kerb-line of Dickens Way and a point 10 metres west of that kerb-line.

### **Dickens Way**

- (a) both sides, between the south-western kerb-line of Oaklands Avenue and a point 10 metres south of the southern kerb-line of Caxton Way;
- (b) the south-east side, between a point 10 metres north-east of the north-eastern kerb-line of Brunel Close and a point 10 metres south-west of the south-western kerb-line of Brunel Close.

**Oaklands Avenue**, the south-west side, between a point 15 metres south-east of the south-eastern kerb-line of Dickens Way and a point 15 metres north-west of the north-western kerb-line of Dickens Way.

## **SCHEDULE 2A**

- Brunel Close** - All properties
- Caxton Way** - All properties
- Dickens Way** - All properties

## **SCHEDULE 2B**

### **Brunel Close**

- (a) the north-west to south-east arm
  - (i) the north-east side, from a point 10 metres south-east of the south-eastern kerb-line of Dickens Way and that kerb-line to a point 10 metres north-west of the north-western kerb-line of the south-west to north-east arm of Brunel Close;
  - (ii) the south-west side, from a point 10 metres south-east of the south-eastern kerb-line of Dickens Way and that kerb-line to a point 10 metres north-west of the north-western kerb-line of the south-west to north-east arm of Brunel Close;

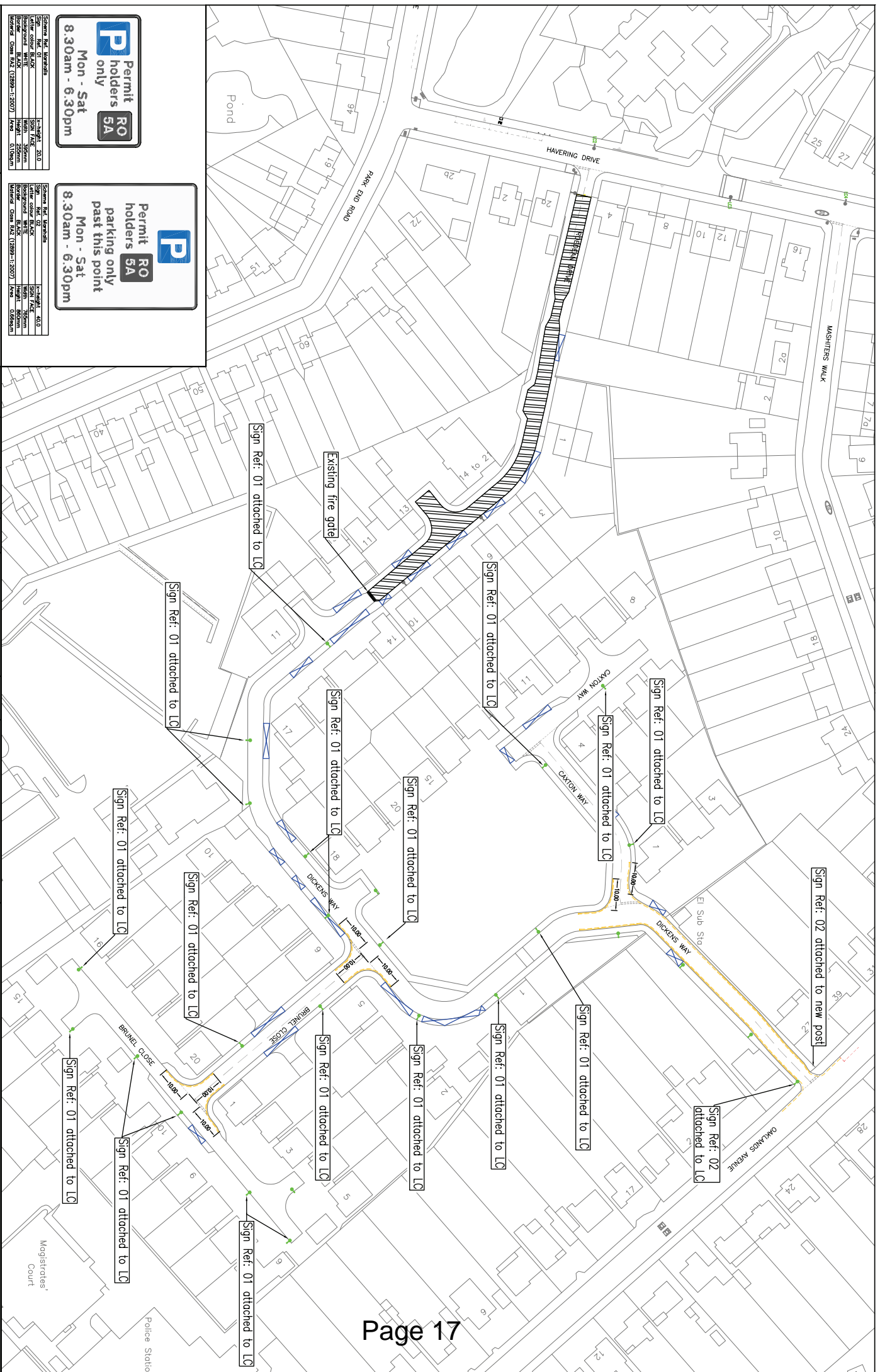
- (b) the south-west to north-east arm
  - (i) the north-west side, from a point 10 metres north-east of the north-eastern kerb-line of the north-west to south-east arm of Brunel Close to its north-eastern extremity, including all sides of the turning heads situated at its north-eastern extremity;
  - (ii) the north-west side, from a point 10 metres south-west of the south-western kerb-line of the north-west to south-east arm of Brunel Close to its south-western extremity;
  - (iii) the south-east side, from its north-eastern extremity to its south-western extremity, including all sides of the turning heads situated at its south-western extremity.

**Caxton Way**

- (a) the north-east to south-west arm
  - (i) the north-west side, from a point 10 metres west of the western kerb-line of Dickens Way to its junction with the south-east to north-west arm of Caxton Way;
  - (ii) the south-east side, from a point 10 metres west of the western kerb-line of Dickens Way to its junction with the south-east to north-west arm of Caxton Way;
- (b) the south-east to north-west arm, the whole street.

**Dickens Way**

- (a) the north-east and south-east sides, from a point 10 metres south of a point opposite the southern kerb-line of Caxton Way to a point 10 metres north-east of the north-eastern kerb-line of Brunel Close;
- (b) the south-east and south-west sides, from a point 10 metres south-west of the south-western kerb-line of Brunel Close to its western extremity;
- (c) the south-west, north-west and north-east sides, from a point 10 metres south of the southern kerb-line of Caxton Way to its western extremity.



**Haverling**  
 LONDON BOROUGH OF  
 STREET CULTURE & COMMUNITY  
 10th FLOOR MERCURY HOUSE  
 MERCURY HOUSE, ROYAL DOCK, RUI 30W  
 TELEPHONE: 020 7708 4343 FAX: 020 7708 4371  
 EMAIL: streetculture@city.gov.uk

**Permit holders 5A**  
 RO  
 Mon - Sat  
 8.30am - 6.30pm

**Permit holders 5A**  
 RO  
 parking only  
 past this point  
 Mon - Sat  
 8.30am - 6.30pm

Scheme Ref: Warrish	10/10/12	20.0
Scheme Ref: OAC	10/10/12	40.0
Background	WHITE	Height: 200mm
Background	WHITE	Height: 300mm
Background	WHITE	Height: 350mm
Background	WHITE	Height: 400mm
Background	WHITE	Height: 450mm
Background	WHITE	Height: 500mm
Background	WHITE	Height: 550mm
Background	WHITE	Height: 600mm
Background	WHITE	Height: 650mm
Background	WHITE	Height: 700mm
Background	WHITE	Height: 750mm
Background	WHITE	Height: 800mm
Background	WHITE	Height: 850mm
Background	WHITE	Height: 900mm
Background	WHITE	Height: 950mm
Background	WHITE	Height: 1000mm

DRAWN BY	BB	CHECKED BY	MP
DATE:	19/10/12	SCALE:	N.T.S.
PURPOSE:	INFORMATION	APPROVED BY:	MP
DMC No:	Q1082-01-101	REVISION:	A

**PROPOSED AT ANY TIME WAITING RESTRICTIONS**  
 (DOUBLE YELLOW LINE)  
 PRIVATE ROAD

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# HIGHWAYS ADVISORY COMMITTEE

19 March 2013

# REPORT

**Subject Heading:**

**ORANGE TREE HILL AND NORTH ROAD, HAVERING-ATTE-BOWER CHANGES TO TRAFFIC CALMING**  
**Outcome of public consultation**

**Report Author and contact details:**

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**The subject matter of this report deals with the following Council Objectives**

Clean, safe and green borough	<input checked="" type="checkbox"/>
Excellence in education and learning	<input type="checkbox"/>
Opportunities for all through economic, social and cultural activity	<input type="checkbox"/>
Value and enhance the life of every individual	<input checked="" type="checkbox"/>
High customer satisfaction and a stable council tax	<input type="checkbox"/>

## SUMMARY

This report sets out the comments received in response to a public consultation for making changes to the pinch point traffic calming features on Orange Tree Hill and North Road to improve conditions and safety for bicycle users.

This scheme is within **Havering Park** ward.

## RECOMMENDATIONS

1. That the Committee having considered the information set out in this report recommends to the Cabinet Member for Community Empowerment that the changes to the pinch points on Orange Tree Hill and North Road be approved for implementation as detailed in this report and shown on the following drawings;
  - QL040/17-101
  - QL040/17-102
  - QL040/17-103
  - QL040/17-104
  - QL040/17-105
  - QL040/17-106
  
2. That it be noted that the estimated cost of the scheme will be £15,000 which will be met from the 2013/14 Transport for London Local Implementation Plan allocation for the Collier Row Casualty-reduction Package.

## REPORT DETAIL

### **1.0 Background**

- 1.1 A casualty-reduction scheme for Orange Tree Hill and North Road was recommended for implementation by the Highways Advisory Committee in December 2010. The scheme included various elements including changes to and additional priority pinch points, relighting of sections of the route, signage de-clutter/ improvements and high grip surfacing.
- 1.2 The pinch points within the scheme (new and altered) were changed from their previous arrangement of being set symmetrically in the road with bypass areas for bicycle users to being set asymmetrically with the bypass areas removed to accommodate the changes.
- 1.3 The two streets involved carry agricultural vehicles which operate with wide and overhanging trailers. This means that a “usual” arrangement of pinch points on alternate side of the road was not possible; otherwise the vehicle/trailer combination would overhang the footways. The asymmetric layout was proposed to try and reduce the reported incidences of drivers being tempted to race oncoming traffic through the features, while accommodating agricultural vehicles.
- 1.4 The original scheme was installed in early 2003 and was based on a casualty study reviewing rates for the 4 years to 2000. In this period, there



were 54 injury collisions along North Road/ Orange Tree Hill and of these, 5 were fatalities and 11 involving serious injury.

- 1.5 The 2010 scheme was based on a review of the 4 years to December 2009 where 15 injury collisions were recorded and of these, 4 were serious. The current scheme was completed in early 2011. Until there are at least 3 years' of casualty data available, Staff would not recommend drawing any conclusions as to the efficacy of the scheme.
- 1.6 Following implementation, Streetcare started to receive several complaints from individual cycle users and club cyclists (including Hainault Roads Club) about the loss of the bypasses and poor driver behaviour when encountering cycle users going through the pinch points. One local cycle user provided video evidence of the behaviour of some drivers he had encountered.
- 1.7 The matter was also raised by local cycle users attending the Council's quarterly Cycle Liaison Group which include individuals and representatives of CTC and the London Cycling Campaign.
- 1.8 The matter was reported to the Highways Advisory Committee on 20<sup>th</sup> September 2011 (Schemes Applications, Item H2) where the Head of Streetcare was authorised to proceed with a review of the layout to assist cycle users passing the pinch points.
- 1.9 Drawings QL040/17/101 to 106 show a series of proposed adjustments which are a combination of allowing cyclists to use short sections of adjacent footways (conversion to shared-use cycle tracks) and reprovision of bypasses (within wide asymmetric islands or verge areas) where space allows.
- 1.10 It is not possible to reintroduce the bypasses at the smaller islands as they would effectively be removed. It is also not possible to bypass at all of the wider islands because of the cost to relocate electrical equipment.
- 1.11 Letters setting out the proposals were hand-delivered to 205 properties along the route on or just after 21<sup>st</sup> November 2012. In addition, ward councillors, HAC members, members of the Council's Cycle Liaison Group, the standard list of consultees (including the emergency services and London Buses), HABCOS and other people who had expressed an interest in the proposals were also sent copies of the consultation information.
- 1.12 In addition, at areas where shared-use cycle tracks are proposed, site notices were erected.
- 1.13 Members of the Council's Cycling Liaison Group were also consulted at one of its regular meetings and the scheme was broadly endorsed.
- 1.14 By the close of consultation on 11<sup>th</sup> January 2013, 10 written responses were received and are summarised in Appendix I.

## **2.0 Outcome of Public Consultation**

2.1 The comments are generally around the following points;

- Concern about the impact of the current layout on cycle users and horse riders;
- Concern that the proposed layout will cause conflicts with pedestrians and could not be used by horse riders;
- Concern about the maintenance of the current and proposed layout;
- Request that the traffic calming is removed in favour of average speed cameras;
- Request that the original scheme is essentially reinstated.
- Comments about the layout of the pinch point island at Orange Tree Hill (near Uplands).

## **3.0 Staff Comments**

3.1 The proposed changes to the existing layout are intended to reduce the risks and concerns expressed by cycle users, but it is recognised that this will not address the horse rider concern. There would be a risk with sections of shared-use cycle track, but it would be for the cycle user to consider the prevailing conditions and behave accordingly.

3.2 The issues of maintenance of the features existed with the original layout and will persist with the current layout. The movement of agricultural vehicles has to be maintained and the need to highlight the features (with bollards) will mean that they are knocked from time to time.

3.3 The main reason behind the original and continued need for traffic calming through the village is one of traffic flow. The route is classified (B175) and carries a great deal of through traffic between Essex and Romford (including the major routes thereafter). To reduce or remove through traffic, it would take substantial agreement between Havering, adjacent boroughs and Essex County Council to close or restrict various routes to through traffic in favour of forcing it to divert to routes with a higher classification such as A113, A112 and A128 (or further afield).

3.4 Until there is at least 3 years' casualty data available, Staff cannot recommend any major changes to the layout or a review, but would suggest that the matter could be revisited during 2014/15 where a funding bid could be made through the usual Local Implementation Plan process if deemed appropriate.

3.5 As set out in the report to the HAC regarding Safety Cameras in December 2012 (Item 6), TfL are maintaining an intervention rate for speed cameras as being locations where there is a casualty rate of 4 KSIs over 3 years, with 2

being speed related. Until there are 3 years' data available from the current scheme, it is doubtful that such an idea could be taken forward.

- 3.6 It is accepted that a system of average speed cameras has the potential to not only manage speed through the village, but to enable a complete removal of "clutter". However, to cover the entire village, 3 "cordon" sites would be required (North Road, Orange Tree Hill and Broxhill Road). Current estimates are for a budget of £100k per camera, per direction which would have to be funded by the borough (£600k). TfL would also seek an annual maintenance charge which has not yet been set. Average speed cameras have not been widely used as permanent arrangements in London and it is likely that application of such a system in Havering-atte-Bower would need a special agreement with TfL. At this stage, it is not considered possible to take the option further.
- 3.7 With the current issues, Staff recommend that the scheme be implemented as designed and a review to take place in 2014/15 to ascertain whether other works are required on a casualty-reduction basis.

## IMPLICATIONS AND RISKS

### **Financial implications and risks:**

This report is asking HAC to recommend to Lead Member the implementation of the above scheme

The estimated cost of implementing the proposals as described above and shown on the attached drawings is £15,000. This cost can be met from the 2013/2014 LIP Allocation for the Collier Row Casualty-reduction package. Spend will need to complete by 31st March 2014 to maximise access to TfL grant funding.

The costs shown are an estimate of the full costs of the scheme, should it be implemented. A final decision would be made by the Lead Member – as regards actual implementation and scheme detail. Therefore, final costs are subject to change.

This is a standard project for StreetCare and there is no expectation that the works cannot be contained within the cost estimate. There is an element of contingency built into the financial estimate. In the unlikely event of an overspend, the balance would need to be contained within the Streetcare Capital Budget.

### **Legal implications and risks:**

The Council may convert existing footways into cycle tracks, by technically "removing" the footway under Section 66(4) of the Highways Act 1980 as amended and "constructing" the cycle track under Section 65(1) of the Highways Act 1980 as amended.

The Council may create new cycle tracks using its powers under Section 65(1) of the Highways Act 1980 as amended.

**Human Resources implications and risks:**

The proposal can be delivered within the standard resourcing within Streetcare, and has no specific impact on staffing/HR issues.

**Equalities Implications and Risks:**

The Council has a general duty under the Equality Act 2010 to ensure that its highway network is accessible to all. Where infrastructure is provided or substantially upgraded, reasonable adjustments should be made to improve access. In considering the impacts and making improvements for people with protected characteristics (mainly, but not limited to disabled people, the young and older people), this will assist the Council in meeting its duty under the Act.

Shared pedestrian and cycle facilities are not always seen by some interest groups as desirable, but given the highway, land space available and it considered appropriate to allow cyclists to legally use off-carriageway sections.

**BACKGROUND PAPERS**

Project Scheme File Ref: QL040/17 Havering-atte-Bower Cycle Bypasses

## SUMMARY OF CONSULTATION RESPONSES

Respondent/	Summary of Comments
Mr Ford London Buses (Operations)	Does not expect scheme to cause London Buses any issues.
Mr Tomlinson	The original pinch point scheme had provision for cyclists and horse riders to pass. When the current scheme was discussed [at a HABCOS meeting], an additional pinch point was put in but cycle access reduced. The pinch points should be left in their current position and returned to the original design which worked perfectly well.
Mr Loveard Townley Cottage	<p>Content with 4 of the pinch points but wishes to comment on two as follows;</p> <ul style="list-style-type: none"> <li>• Oak Hill Road/ Home Farm [Drawing QL040/17-106]. For this to work, the verge will need to be strimmed back far more than it currently is as vegetation often protrudes over the footway. It is essential that the back edging kerb and area of tarmac be reinstated following connection of a feeder pillar.</li> <li>• Orange Tree Hill, by Uplands [Drawing QL040/17-101]. Following construction of the islands in 2010, the intended priorities were changed, leaving the wider island on the side where traffic has priority. Could funding be found to reduce this island and extend the narrow island and provide a cycle bypass on the wider side?</li> </ul>
Mr Potter North Road	<p>The original [pre-2010] scheme failed to restrict the excessive speed of the majority of vehicles. It was unfit for purpose. No arrangements were made for cleaning the bypasses or signs. The aperture was not wide enough for agricultural vehicles.</p> <p>Suggests that vans and 4x4s run up the centre of the road at 45-50mph, straddling the line and bullying their way through. The approaches should be double white lines with metal studs.</p> <p>The revised arrangement brought chaos with construction with limited access. The pinch point at the end of Orange Tree Hill [QL040/17-101] was originally installed with uphill traffic giving way. The scheme was gilding the lily of a scheme which was not fit for purpose.</p> <p>The current suggesting is to allow cyclists and horse riders to use the footpath which is illegal and would impact on pedestrians, pensioners and mothers with push chairs who will be forced into the road.</p> <p>The speeding problem will only be solved with police using a radar</p>

	<p>gun or a Gatso camera with fines and licence points.</p> <p>Resident suggests that the timing for the notice at Christmas and New Year was an attempt to slip it under the radar. The scheme is not fit for purpose in calming the traffic. Resident has lived in the village since 1947 and knows about the increase in volume, speed and size of traffic and sat nav has contributed.</p>
<p>Mr Heap Broxhill Road</p>	<p>As with many in the village, resident is concerned about loss of satisfactory escape lanes for cyclists and most particularly horses. Can appreciate what has been done for the scheme, but does not see that making cyclists use the pavement is prudent. More importantly, with no north-south bridleway, the proposals do not take account horse riders as they are not practical and user cannot cope with overhanging vegetation and would have to carry on using the centre of the road.</p> <p>Considers that the chicanes by the village green in the heart of the conservation area was inappropriate in the first place. Resident appreciates safety worries, but they are an eyesore. They contradict the Council's own Conservation Area Character Appraisal recommendations which were to reduce the signage and clutter around the village green.</p> <p>Resident's first wish is for the chicanes to be removed and replaced with average speed cameras which would be the right solution and visually acceptable. Resident understands there are moves afoot to allow receipts from such to be kept locally and so it would be better to save costs now and use them for average speed cameras.</p> <p>If speed cameras are not possible, then the chicanes should be narrowed to leave a much larger gap for cyclists and horses.</p> <p>In Orange Tree Hill, the first pinch point has the vehicular opening on the wrong side and should be corrected.</p>
<p>Ms Gates HABCOS</p>	<p>Writing on behalf of the Havering-atte-Bower Conservation Society.</p> <p>The proposals have been discussed at a HABCOS meeting and AGM. Appreciates the council were trying to make changes to the pinch points to give a bypass for cycles and possibly horse riders, there are still some reservations.</p> <p>The reason for requesting the changes is because cyclists and horse riders are "challenged" by car drivers, even when cars should give way and giving concerns about potential accidents. Although the proposals would give an alternative, it would put pedestrians at risk. Some areas of Orange Tree Hill and North Road have very narrow pavements and the proposals could move accident potential from the road to the pavements, especially where vegetation is over growing.</p>

	<p>Although there has been no loss of life since the pinch points have been put in place, they have been the cause of many accidents and incidents between road users. Rather than spend money on a system that is not particularly successful or desired, would it not be better to spend money on a system which would work better.</p> <p>HABCOS is constantly being asked by residents about average speed cameras. Would this not be a better way of calming traffic? From a conservation point of view, the current system is unsightly and doesn't compliment the conservation area or enhance the beauty of the village in any way.</p>
Mr Hardcastle	<p>Over the last 3 years considers that the route has become very dangerous for cyclists. The first scheme was OK as it allowed for cyclists to pass the narrowing without danger from cars, but then the cycle lane was taken away.</p> <p>User has sent several videos showing how the layout is dangerous, but it has not been taken seriously. Liberty cottages is a disaster as cars do not stop coming up the hill and play chicken with cyclists.</p> <p>Why haven't speed humps been used instead of making people try to beat each other through the gap? Does not believe that people actually wanted cycle lanes to be shut off.</p> <p>Comments on how the original layout was OK for cyclists and supplies video footage.</p>
Mr Dimond	<p>Resident considers that the pinch points should be replaced as they were before and as a resident and cyclist cannot understand why they were altered in the first place.</p>
Dr Miller Sims Close	<p>Requests copies of proposals and recommends a number of organisations to consult [which Staff have].</p> <p>Resident's experience is that 1 in 3 drivers will give way where the cyclist has right of way. This has led to many dangerous occurrences particularly where fast traffic travelling down North Road fails to give way. Vehicles tend to give way where other vehicles are following cyclists, but sometimes people try and overtake.</p> <p>The pinch point at the bottom of North Road does not allow cyclists enough visibility to see oncoming traffic.</p>
Mr Gwinn JP North Road	<p>The intended changes of sharing a narrow pavement puts cyclists and pedestrians at risk, especially as this pavement is used to take children to Dame Tipping School in North Road. Additionally for much of the year the hedgerows are overgrown resulting in further</p>

width restrictions on pavement access.

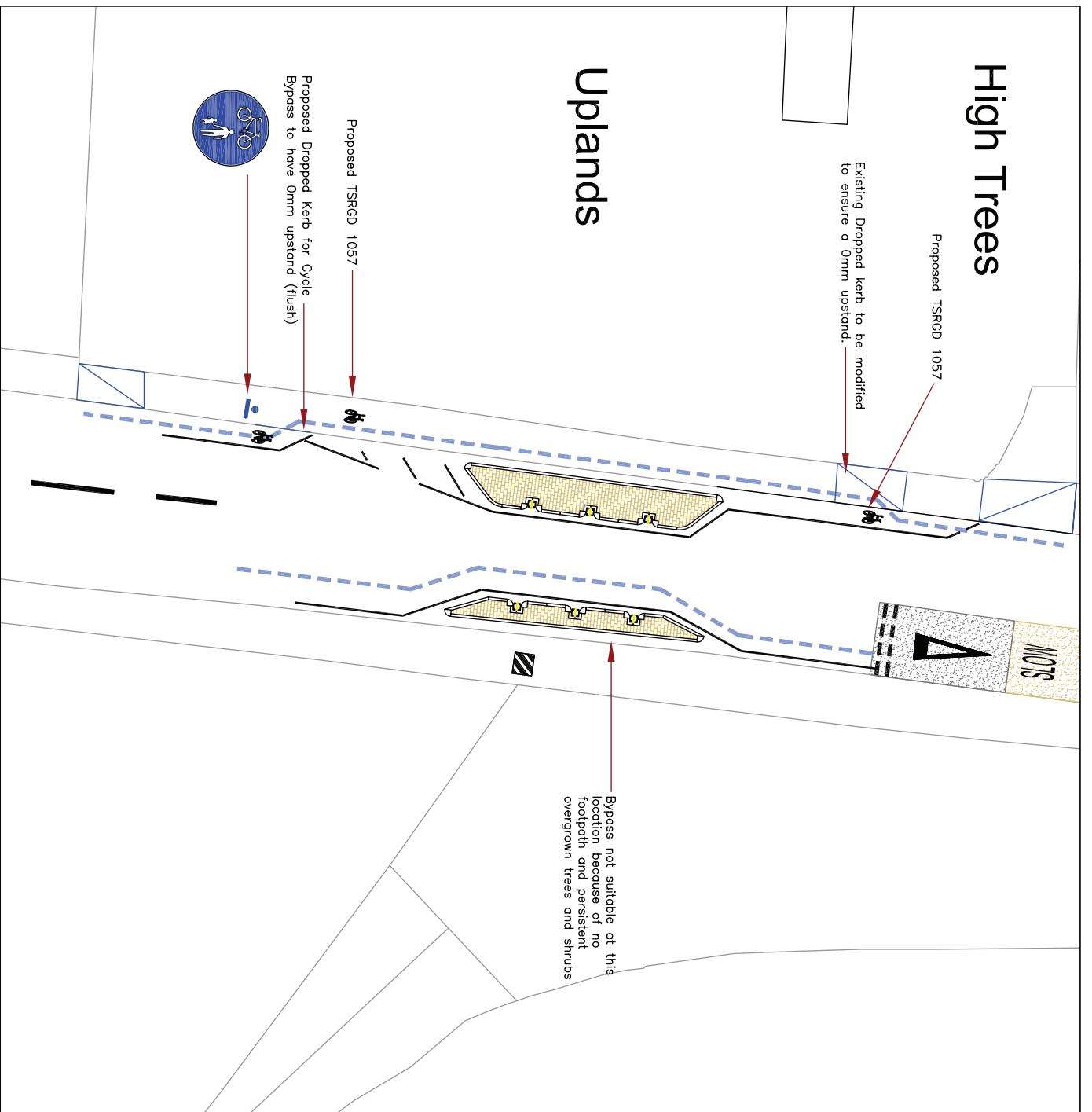
The pinch points are dangerous for horses, riders and vehicles to use. Horses cannot use pavements due to lack of headroom and it is inappropriate to share the pavement with pedestrians and cyclists. Currently horses frequently cause congestion at the pinch points and hold up the traffic and consequently drivers become impatient and try and overtake when it is not safe to do so.

The pinch points are not maintained and are not fit for purpose. Many uprights have no lights, are dirty and broken. Many fell into disrepair after a few weeks following installation. Agricultural vehicles have broken drain covers. The pinch point in Orange Tree Hill is arranged on the wrong side.

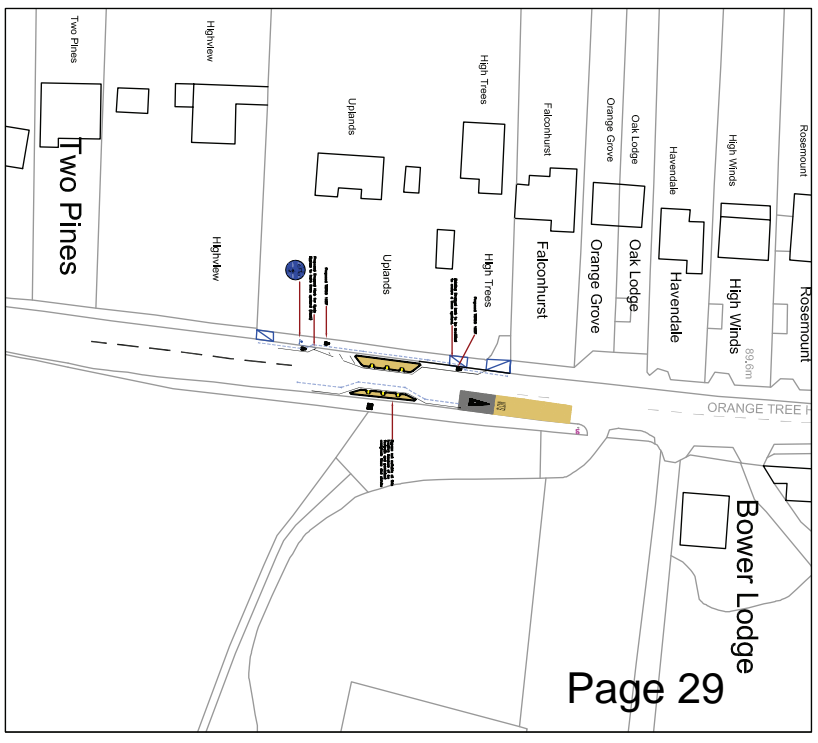
The solution is average speed cameras set at 30mph at the beginning and end of the village which would solve all of the difficulties and not need continual maintenance.



# High Trees

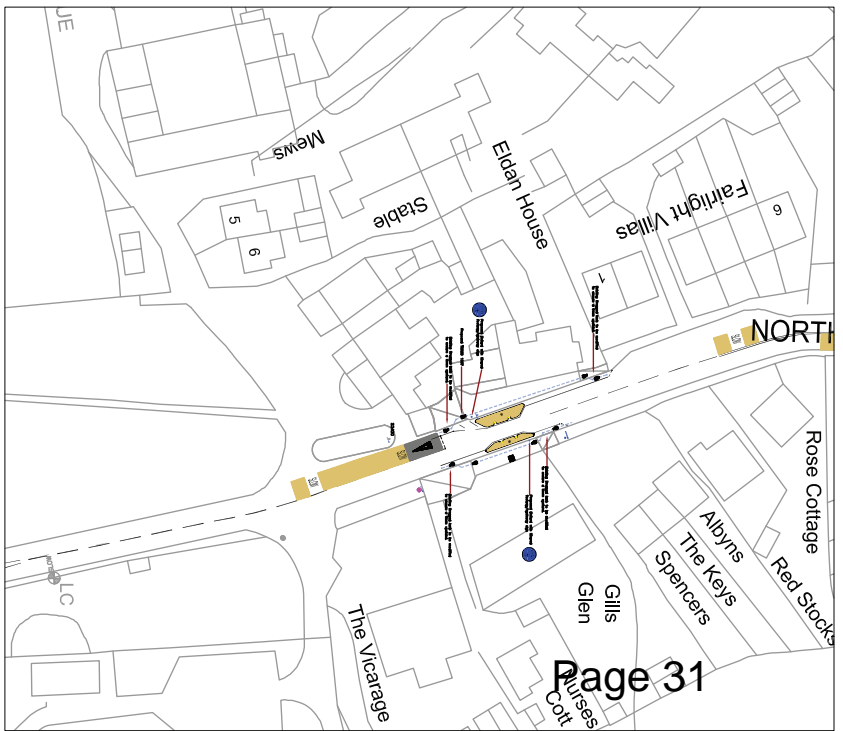
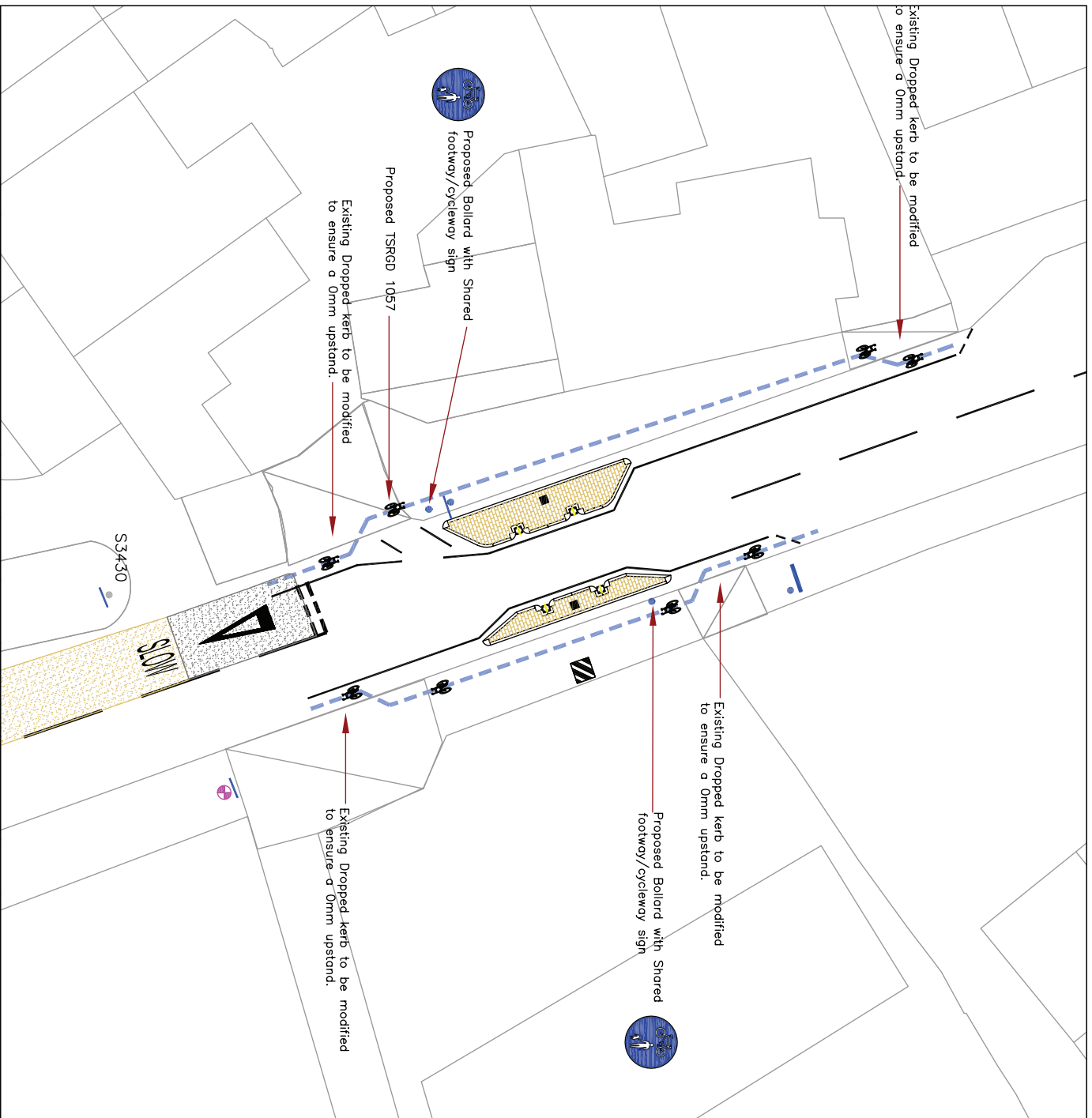



- TSRGD 1003 (100mm)
- TSRGD 1057 (100mm)
- Cycle Route



STREETCARE CULTURE & COMMUNITY 10th FLOOR MERCURY HOUSE MERCURY GARDENS, ROYAL DARTMOUTH TELEPHONE No: 01708 434343 FAX No: 01708 433721 Email: streetcare@overgreen.co.uk		JOB TITLE Hoovering - after - Bower North Road		DRAWN BY TAS		CHECKED BY		APPROVED BY		DATE	
DRAWING TITLE Cycle By - Pass		DATE April 12		SCALE 1:200		PURPOSE Consultation		DRAFT		DATE	
DWC No. Q1.040/17 - 101		REVISION: -		REVISION		AMENDMENT				DATE	
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- TSRGD 1003 (100mm)
-  TSRGD 1057 (100mm)
- - - - - Cycle Route

STREETCUBE CULTURE & COMMUNITY TRAFFIC & ENGINEERING 10th FLOOR, AMERICAN HOUSE 100, NORTH ROAD, HOVERING TEL: 01708 434243 FAX: 01708 437271 EMAIL: steve@streetcube.co.uk	
JOB TITLE	Hoovering - after-Bower
DATE	April 12
SCALE	1:200
PURPOSE	Consultation
DWG No.	QUAD/17 - 103
REVISION	-
REVISION	AMENDMENT
DATE	
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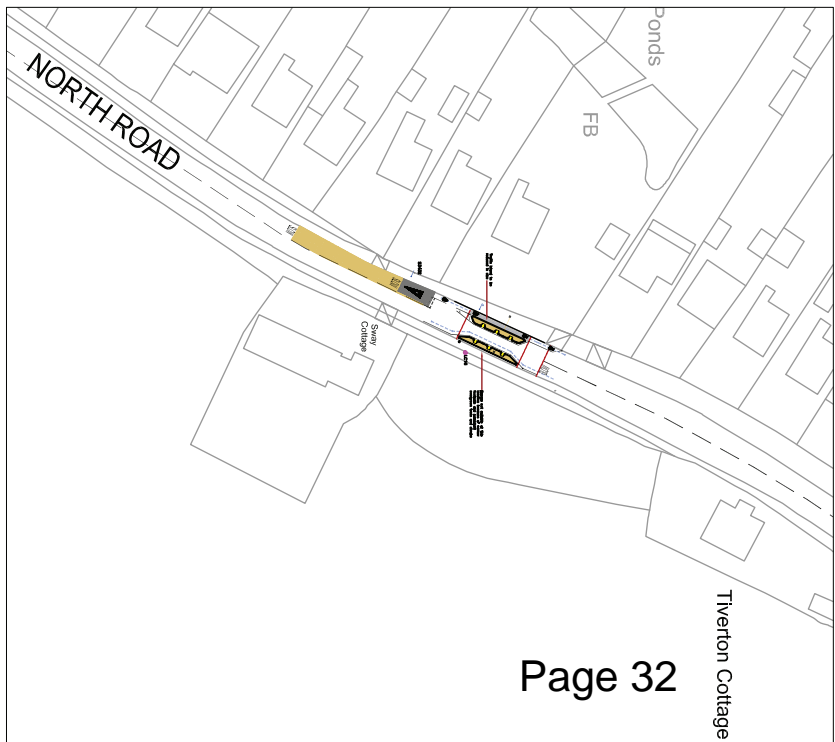
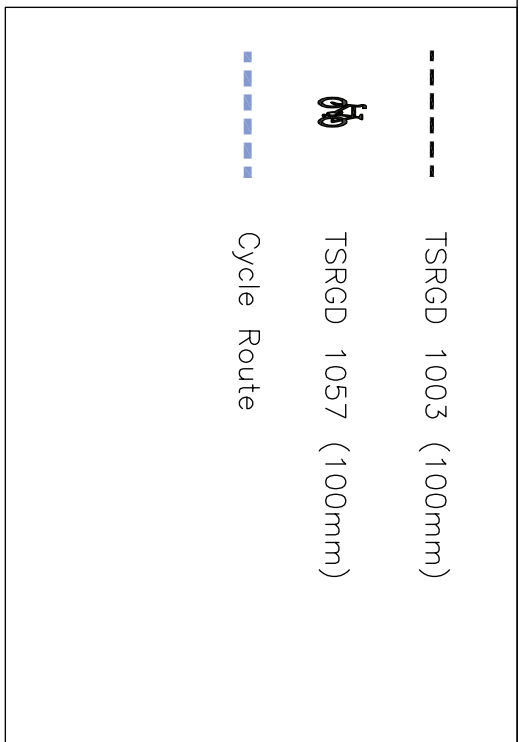
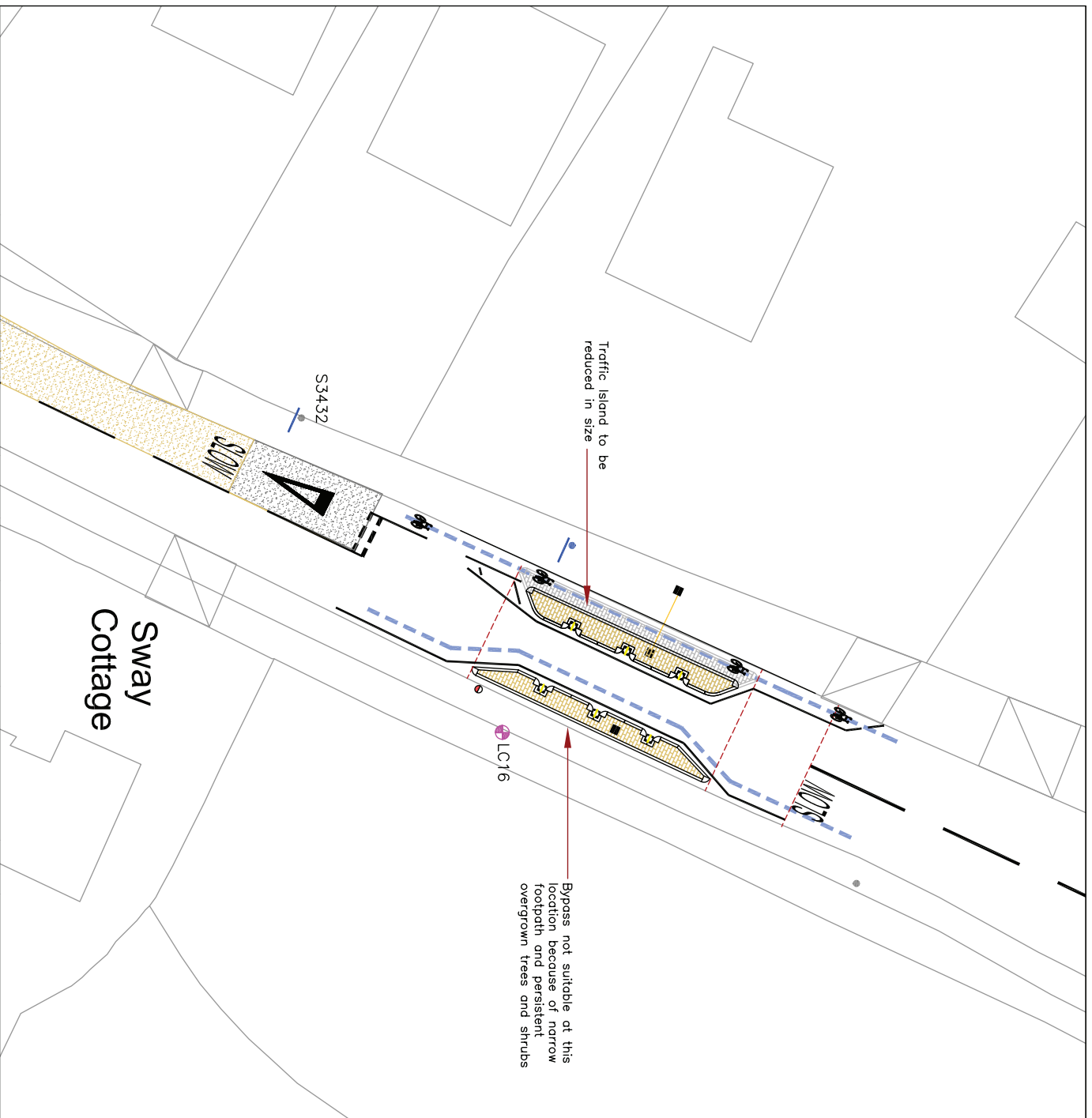
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MS		
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PURPOSE	CONSULTATION	
DWG No.	QUAD/17 - 103	
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REVISION	AMENDMENT	
DATE		

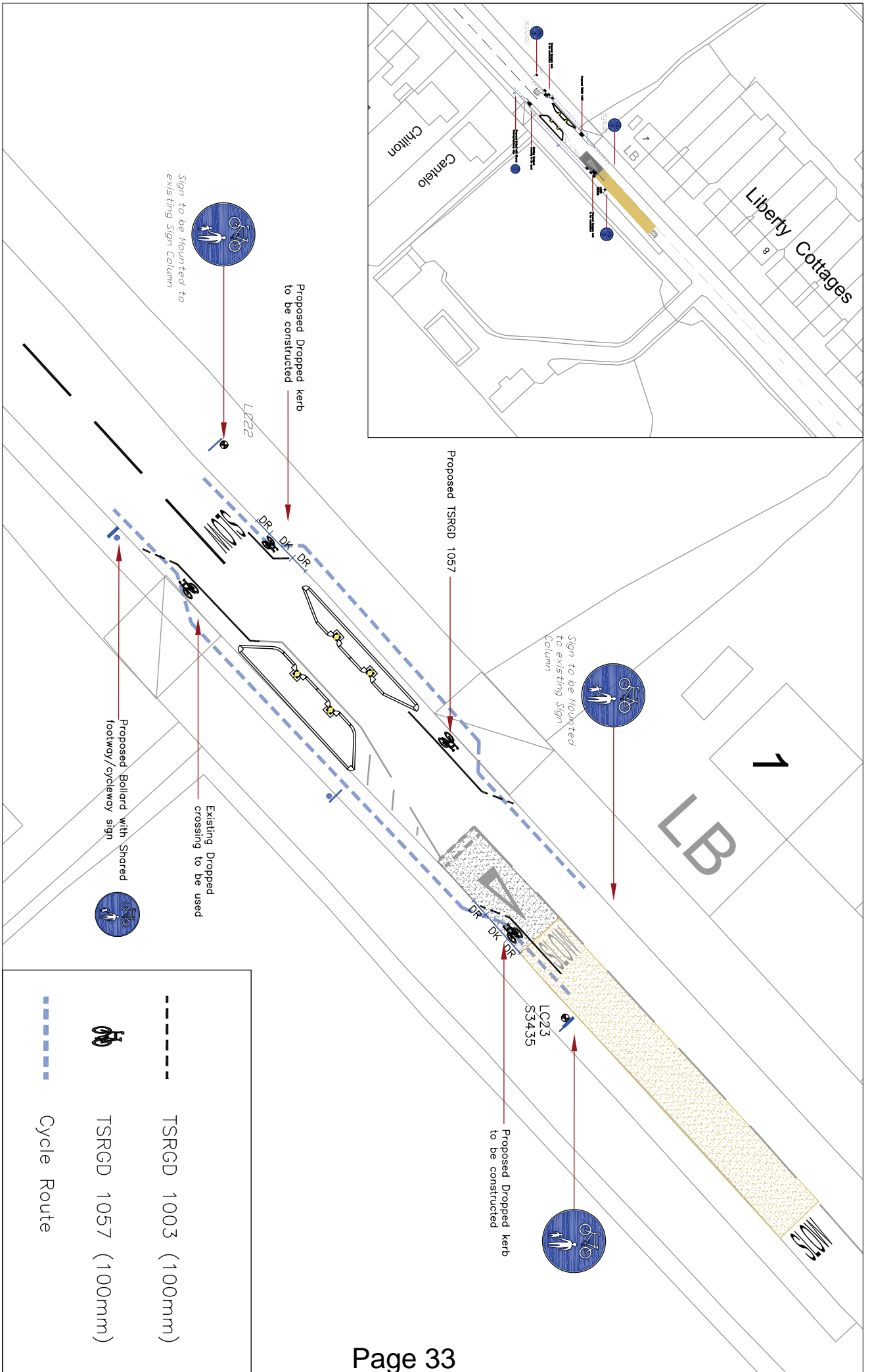
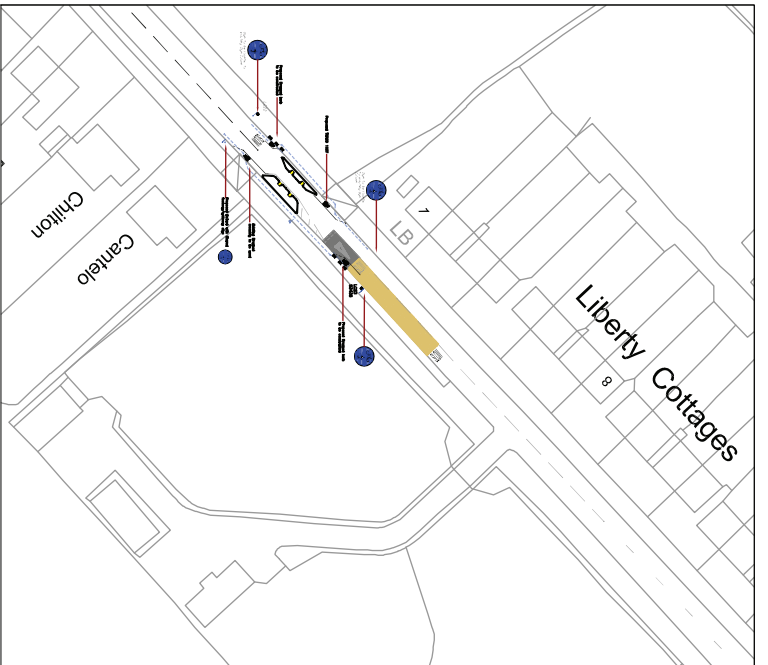
<b>JOB TITLE</b>	Hovering - after-Bower
<b>North Road</b>	
<b>REVISION</b>	By - POSS

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<b>DATE:</b>	April 12	<b>SCALE:</b>	1:200		
<b>PURPOSE:</b>	Consultation	<b>STATUS:</b>	DRAFT		
<b>DWG No:</b>	Q1040/17 - 104	<b>REVISION:</b>	-		

<b>REVISION</b>		<b>AMENDMENT</b>		<b>DATE</b>	

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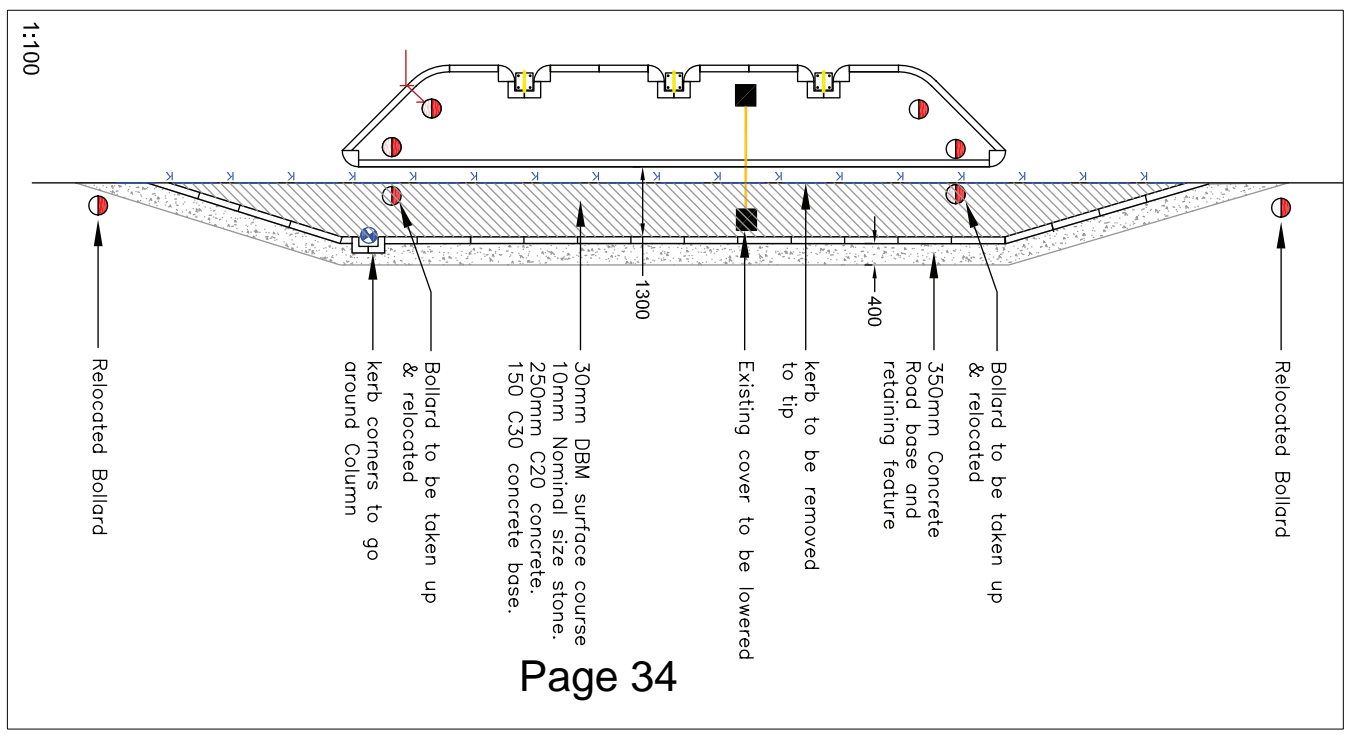
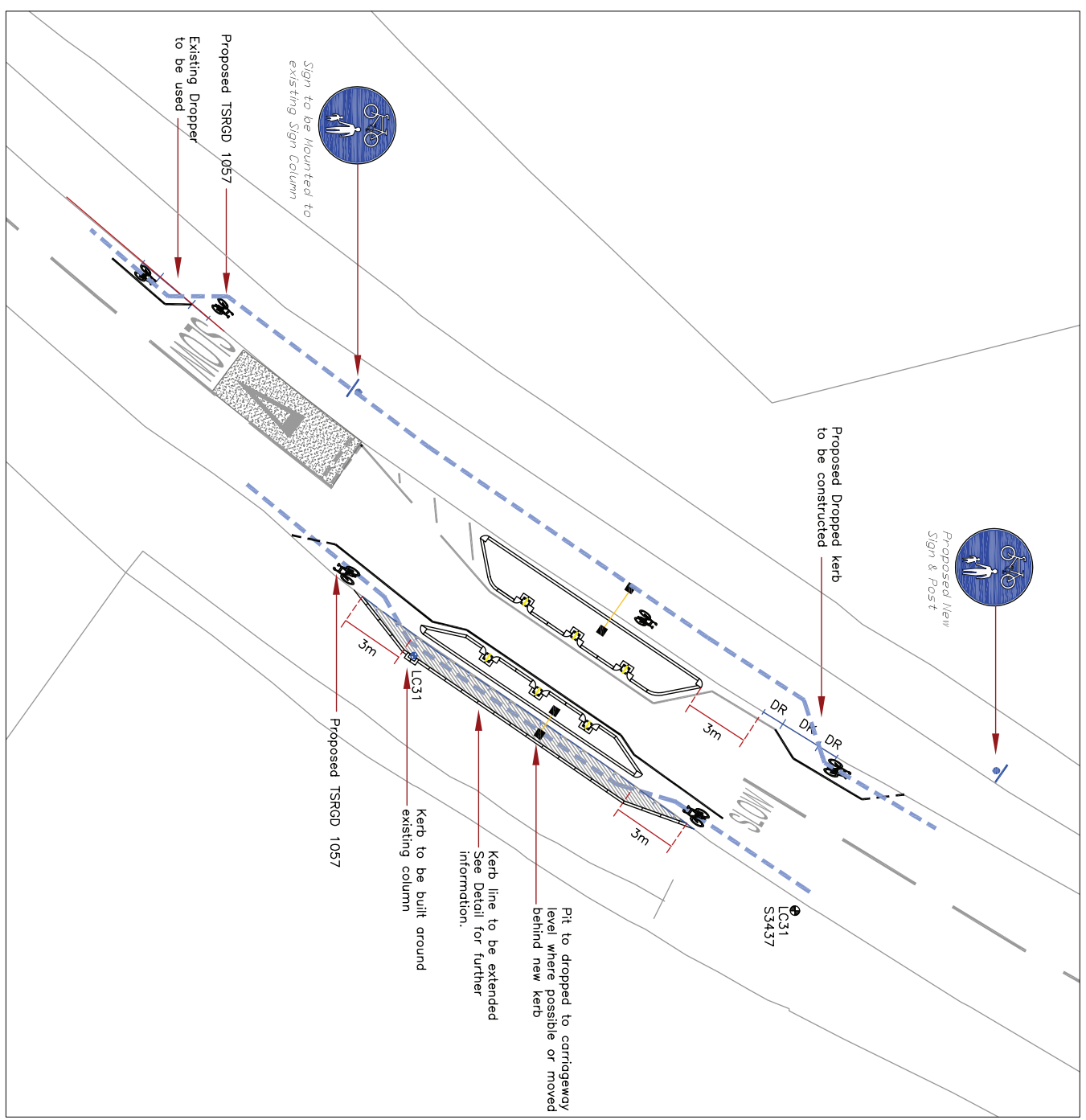
STREETCARE CULTURE & COMMUNITY  
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JOB TITLE  
 Hovering-offite-Bower  
 DRAWING TITLE  
 North Road  
 Cycle By-Pass

DRAWN BY: MB  
 CHECKED BY:  
 APPROVED BY:  
 DATE: April 12  
 SCALE: 1:200  
 PURPOSE: Consultation  
 DWG No: Q1008/17 - 105

REVISION	AMENDMENT	DATE

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<p>STREET/PAVE CULTURE &amp; COMMUNITY TRAFFIC &amp; ENGINEERING 100 FLORIAN MERRICK HOUSE 100Y MANSOURIAH RD, NORTH RIVER TEL: 011 708 4428 FAX: 011 708 4371 EMAIL: street@roadfind.gov.za</p>		<p><b>JOB TITLE</b> Hoovering - after-Bower <b>North Road</b></p>		<p><b>DATE:</b> April 12 <b>SCALE:</b> 1:200 <b>PURPOSE:</b> Consultation <b>DMC No. QUD40/17 - 106</b></p>		<p><b>DRAWN BY</b> TAS</p>		<p><b>CHECKED BY</b></p>		<p><b>APPROVED BY</b></p>	
<p><b>PLANNING TITLE</b> Cycle By-Pass</p>		<p><b>REVISION</b> -</p>		<p><b>REVISION</b> -</p>		<p><b>REVISION</b> -</p>		<p><b>REVISION</b> -</p>		<p><b>REVISION</b> -</p>	
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# HIGHWAYS ADVISORY COMMITTEE

19 March 2013

# REPORT

**Subject Heading:**

**HIGHWAY SCHEMES APPLICATIONS**

**Report Author and contact details:**

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Principal Engineer  
01708 433751  
mark.philpotts@havering.gov.uk

**The subject matter of this report deals with the following Council Objectives**

Clean, safe and green borough	<input checked="" type="checkbox"/>
Excellence in education and learning	<input type="checkbox"/>
Opportunities for all through economic, social and cultural activity	<input checked="" type="checkbox"/>
Value and enhance the life of every individual	<input type="checkbox"/>
High customer satisfaction and a stable council tax	<input type="checkbox"/>

## SUMMARY

This report presents applications for new highway schemes for which the Committee will make recommendations to the Head of StreetCare to either progress or the Committee will reject.

## RECOMMENDATIONS

1. That the Committee considers that the Head of StreetCare should proceed with the detailed design and advertisement (where required) of the highway schemes applications set out the attached Schedule, Section A – Scheme Proposals with Funding in Place.
2. That the Committee considers the Head of StreetCare should not proceed further with the highway schemes applications set out in the attached Schedule, Section B - Scheme proposals without funding available.
3. That the Committee notes the contents of the Schedule, Section C – Scheme proposals on hold for future discussion.
4. That it be noted that any schemes taken forward to public consultation and advertisement (where required) will be subject to a further report to the Committee and a decision by the Cabinet Member for Community Empowerment if a recommendation for implementation is made.
5. That it be noted that the estimated cost of implementing each scheme is set out in the Schedule along with the funding source. In the case of Section B - Scheme proposals without funding available, that it be noted that there is no funding available to progress the schemes.

## REPORT DETAIL

### **1.0 Background**

- 1.1 The Highways Advisory Committee receives all highway scheme requests; so that a decision will be made on whether the scheme should progress or not before resources are expended on detailed design and consultation.
- 1.2 Several schemes are funded through the Transport for London Local Implementation Programme and generally the full list of schemes will be presented to the Committee at the first meeting after Annual Council, unless TfL make an early funding announcement, in which case the list can be provided early. Some items will be presented during the year as programmes develop.
- 1.3 There is also a need for schemes funded by other parties or programmes (developments with planning consent for example) to be captured through this process.



- 1.4 Where any scheme is to be progressed, then the Head of StreetCare will proceed with the detailed design, consultation and public advertisement (where required). The outcome of consultations will then be reported to the Committee which will make recommendations to the Cabinet Member for Community Empowerment. Where a scheme is not to be progressed, then the Head of StreetCare will not undertake further work.
- 1.5 In order to manage this workload, a schedule has been prepared to deal with applications for new schemes and is split as follows;
- (i) Section A - Scheme Proposals with Funding in Place. These are projects which are fully funded and it is recommended that the Head of StreetCare proceeds with detailed design and consultation.
  - (ii) Section B - Scheme proposals without funding available. These are requests for works to be undertaken where no funding from any source is identified. The recommendation of Staff to the Committee can only be one of rejection in the absence of funding. The Committee can ask that the request be held in Section C for future discussion should funding become available in the future.
  - (iii) Section C - Scheme proposals on hold for future discussion. These are projects or requests where a decision is not yet required (because of timing issues) or the matter is being held pending further discussion should funding become available in the future.
- 1.6 The schedule contains information on funding source, likely budget (as a self-contained scheme, including staff design costs), the request originator, date placed on the schedule and a contact point so that Staff may inform the person requesting the scheme the outcome of the Committee decision.

## IMPLICATIONS AND RISKS

### **Financial implications and risks:**

The estimated cost of each request or project is set out in the Schedule for the Committee to note.

The costs shown are an estimate of the full costs to implement a scheme should it be ultimately implemented. It should be noted that further decisions are to be made following a full report to the Committee and with the Cabinet Member approval process being completed where a scheme is recommended for implementation.

**Legal implications and risks:**

Many aspects of highway schemes require consultation and the advertisement of proposals before a decision can be taken on their introduction.

Where a scheme is selected to proceed, then such advertisement would take place and then be reported in detail to the Committee so that a recommendation may be made to the Cabinet Member for Community Empowerment.

With all requests considered through the Schedule, a formal set of Recommendations and a record of the Committee decisions are required so that they stand up to scrutiny.

**Human Resources implications and risks:**

None.

**Equalities implications and risks:**

The Council has a general duty under the Equality Act 2010 to ensure that its highway network is accessible to all. Where infrastructure is provided or substantially upgraded, reasonable adjustments should be made to improve access. In considering the impacts and making improvements for people with protected characteristics (mainly, but not limited to disabled people, the young and older people), this will assist the Council in meeting its duty under the Act.

Decisions need to be made which are in accordance with equalities considerations, the details of which will be reported in detail to the Committee so that a recommendation may be made to the Cabinet Member for Community Empowerment.

**BACKGROUND PAPERS**

None.

**London Borough of Havering  
Traffic & Engineering - StreetCare  
Highway Schemes Applications Schedule**

**Highways Advisory Committee  
19th March 2013**

Item Ref	Location	Description	Officer Advice	Funding Source	Likely Budget	Scheme Origin/ Request from	Date Requested/ Placed on List	CRM / Contact
<b>SECTION A - Highway scheme proposals with funding in place</b>								
Nothing reported this month								
<b>SECTION B - Highway scheme proposals without funding available</b>								
TH1	Minster Way, Hornchurch	Speed humps required to stop speeding and amount of traffic using street or no right turning into Wingletye Lane	Feasible but unfunded	None	£25k / £5k	Resident	18/02/2013	1152162
TH2	St Leonards Way	Speed humps to slow traffic down. Residents' cars have been hit by speeding traffic.	Feasible but unfunded	None	£30k	Resident	11/03/2013	1152640
<b>SECTION C - Highway scheme proposals on hold for future discussion (for Noting)</b>								
Nothing reported this month								

1152640

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